 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	1 st November 2017
	REPORT OF:	HEAD OF PLACES & PLANNING
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AGENDA ITEM:	7	WARD: Horley East

APPLICATION NUMBER:	17/01839/F	VALID:	21 st August 2017
APPLICANT:	Langshott Farms Ltd	AGENT:	Brookes Architects Ltd
LOCATION:	KERRIEMUIR & WHEATRIDGE, LANGSHOTT, HORLEY		
DESCRIPTION:	Demolition of Kerriemuir and Wheatridge on Langshott Wood and development of a single detached property comprising eight apartments.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for the demolition of the existing semi-derelict pair of semi-detached properties on the northern side of Langshott and the erection of a single block containing 8 apartments with associated access, parking and landscaping.

The site is within the urban area and adjoins the North East Sector development ("The Acres") to the north and east. Given its location in the urban area, there is no in principle objection to the proposed residential development.

The application follows previous proposals on the site which have been refused principally due to a cramped over development of the site, adverse impact on neighbour amenity and highway concerns. This application seeks to overcome this by proposing a single residential block (thus avoiding development on the rear of the site) and improvements to the design and layout to respond to the character of the area.

Whilst the proposed building would be larger than that which it replaces, its design, scale and massing, and the layout of the site are considered to be appropriate. The staggered building line and spacing to the boundaries would overcome previous concerns about the erosion of the spacious, verdant character of the streetscene of Langshott and would allow for retained and enhanced frontage landscaping. The form of the building would sit comfortably alongside neighbouring dwellings and the height reflects an appropriate transition between the adjacent cottages and the more modern development on The Acres. Design detailing would follow the principles set out in the Horley Design Guide SPD and respect the locally distinctive Wealden vernacular. The Conservation Officer has

reviewed the proposals given sensitivity in relation to the setting of nearby listed buildings and has raised no objection.

Adequate separation would be retained between the proposed building and neighbouring properties such that, whilst there would be some change in relationship, the development would not cause serious detriment to their amenities. Possible noise and disturbance from the access road and parking area on neighbouring gardens could be adequately mitigated through boundary treatments and landscaping, both of which are recommended to be secured through condition.

The Tree Officer has raised no objection and has confirmed that, subject to conditions, the development can be achieved without adverse impact on the two protected oak trees on the frontage of the site. The application has been considered by the Highway Authority who has raised no objection subject to conditions.

The proposal would make a contribution to local housing supply on an urban site, with consequent economic, financial and social benefits – including CIL contributions.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

County Highway Authority: No objection on highway safety or capacity grounds subject to conditions. The response of the CHA provides the following detailed informative commentary:

Surrey County Council (SCC) is currently undertaking highway improvement works on Langshott, as part of the Horley North East Sector Development. These works include the provision of a new footway along the northern side of the road, and a shared surface passing place directly in front of the application site, to facilitate the new Fastway Bus Service. A traffic regulation order (TRO) has been made to introduce double yellow line parking restrictions within the passing place, to ensure that it is kept clear of parked vehicles at all times.

The construction of the proposed vehicular access to serve the new development is likely to interfere with the provision of the new footway and passing place that are currently being constructed. Therefore, to minimise any disruption to these works, the applicant has made a minor adjustment to the alignment of the proposed access road, so that it would tie in better with the SCC scheme. The proposed access has been repositioned very slightly to the west where it meets the highway, so that it is located wholly within the passing place and does not cross over the new footway. While the position of the proposed access shown on the amended Proposed Site Plan (drawing no. 4798-3-45-B) is generally acceptable, the exact location and detailed design of the access might need to be amended slightly during the Mini Section 278 agreement process. For this reason, condition 1 requires the proposed access to be constructed in accordance with a scheme to be submitted.

The applicant has also relocated the bin store to the front of the site, to discourage refuse vehicles from entering the site and having to reverse back out onto the public highway. The passing place is long enough to accommodate a refuse vehicle and a car, and the double yellow lines would not prohibit vehicles from loading or unloading. A refuse vehicle would therefore be able to wait in the passing place while it services the development, leaving sufficient space for a bus to pass.

The site boundary is set back approximately 2m from the edge of the passing place, and condition 1 [in the CHA response] requires the provision of a visibility zone to the west of the access. This visibility splay should be measured from a distance of 2.4m back from the edge of the passing place, and should be kept permanently clear of any obstruction over 1.05m high. Vehicles exiting the site access would therefore have adequate visibility of vehicles pulling into or out of the passing place, and of pedestrians walking across the passing place/footway.

The applicant should be aware that if any damage is caused to the new passing place, footway or verge during construction of the proposed development, these will need to be fully repaired or reinstated at their own expense. The passing place will need to be kept clear of construction vehicles at all times, and appropriate traffic management measures

will need to be put in place, to ensure the safe operation of the bus service, and the safe passage of pedestrians.

The CHA acknowledges that the proposed development of eight apartments would lead to an increase in vehicular movements to and from the site, when compared with the existing two dwellings. However, given the small scale of the proposed development, the increase in traffic would be negligible and would not lead to a material impact on the safety and operation of the adjoining public highway.

Tree Officer: No objection subject to conditions. Key extracts from response as follows:

The trees at the front of the site form an important part of the sylvan setting of Langshott lane. The oaks are mature and visually significant in the local and wider landscape and are typical of the historic 'low weald' landscape character that exists in this locality.

The application has been supported by qualified Arboricultural information compiled by an arboricultural consultant (AC) in accordance with the advice guidelines and recommendation contained within British Standard 5837 trees in relation to design demolition and construction-Recommendations. I have relied on externally viewing the site on the 13th September 2017 and my previous knowledge and the arboricultural details submitted to reach my conclusion in respect of this proposed development relating to the arboricultural and landscape matters.

The proposed development will result in the loss of a number of trees all of which have been assessed and categorised adopting the criteria from section 4 and table 1 of the above standard. The trees lost to this development comprise of self-sown trees in 'C' category. The trees are self-sown and many have established through neglect and lack of maintenance.

The supplied Arboricultural Implications Assessment and Tree Protection Plan (TPP) provide sufficient information at this point to enable an informed and balanced decision to be taken on tree loss and retention. Tree protection measures are clearly set out within the TPP and areas where specialist construction techniques, qualified Arboricultural supervision and monitoring will need to be identified once services routing are known.

The development provides an opportunity to improve and enhance existing landscape to the benefit of both the wider and immediate community. Meaningful landscape and replacement tree planting will be secured again through condition and is required in order to make provision for the communal use of land by future occupants of the development.

Conservation Officer: No objection subject to conditions regarding materials and landscaping. Comments as follows:

This plot forms part of the setting of the listed building setting (The Farmhouse and barn, Tanyard Farmhouse and Barn) as defined in the Horley Design Guide. The group is defined by tile hanging cottages, grass verges and hedge frontages. It is considered the proposal is in keeping with the character and architectural conventions of the group.

Horley Town Council: No objections

UK Power Networks: No objections

Surrey Lead Local Flood Authority: Satisfied that proposed drainage scheme meets necessary requirements and raises no objection subject to conditions

SES Water: No comments

Representations:

Letters were sent to neighbouring properties on 25th August 2017 and on the amended plans on 29th September 2017. 8 responses, including duplicate responses on the amended plans, have been received raising the following issues:

Issue	Response
Out of character with surrounding area	Paragraphs 6.4 to 6.11 and conditions 4, 5 and 7
Overdevelopment	Paragraphs 6.4 to 6.11
Overlooking and loss of privacy	Paragraphs 6.12 to 6.18 and condition 15
Noise and disturbance	Paragraphs 6.15 to 6.18 and condition 14
Inadequate parking	Paragraph 6.19 to 6.24 and conditions 9 and 10
Increase in traffic and congestion	Paragraph 6.19 to 6.24
Hazard to highway safety	Paragraph 6.19 to 6.24 and conditions 9, 10, 11, 12 and 13
Harm to wildlife habitat	Paragraph 6.35 and condition 3
Loss of private view	Not a material planning consideration

1.0 Site and Character Appraisal

- 1.1 The site comprises a pair of vacant semi-derelict, modestly sized, 1.5 to 2 storey semi-detached properties, which are set within a large plot on the northern side of Langshott. The site has significant landscaping along the Langshott frontage, including two mature oak trees which are subject to a protection order (RE1245), which provides screening and contributes to the character of Langshott. The two semi-detached dwellings are set back significantly from the road frontage. Various small scale outbuildings exist within the rear gardens. Two accesses presently serve the site.
- 1.2 Development along Langshott is typified by detached and semi-detached buildings. On the northern side, buildings are set back substantially from the road behind grass verges and frontage landscaping including trees and native hedges, with informal gravel driveways providing access from Langshott. Buildings are well spaced, with ample separation to boundaries reinforced by further landscaping. Adjacent to the site are a number of recently built properties forming part of the

North West Sector development: whilst of more recent construction, these respect the frontage along Langshott, being significantly set back and retaining frontage landscaping.

- 1.3 Whilst the site is located within the urban area, these features provide Langshott with an informal, rural, landscape dominated appearance and character. This stretch of Langshott forms part of the historic setting of the Grade II listed buildings in the locality – The Farmhouse, Easter Barn and Little Barn which are a short distance to the west and Tanyard Farm which is a short distance to the east.
- 1.4 As a whole, the application site extends to approximately 0.22ha.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was sought on an earlier iteration of the proposals. In response to advice given, the proposed building has been reduced in width and depth which has allowed for greater separation to the boundary with No.4 Langshott Cottages. Design improvements and greater landscaping are also proposed.
- 2.2 Improvements secured during the course of the application: Realignment of the access onto Langshott to reflect on-going footway improvement works. Relocation of bin store to enable appropriate servicing by refuse vehicles. Improvements to elevation through reduction in eaves height of main gables and minor amendments to ground floor fenestration.
- 2.3 Further improvements to be secured through planning conditions or legal agreement: Conditions are recommended to control landscaping, materials, tree protection and highway matters. Additional conditions are also recommended in the interests of neighbour amenity, including obscuring several first floor side facing windows.

3.0 Relevant Planning and Enforcement History

3.1	15/02543/F	Demolition of houses and outbuildings. Erection of two pairs of semi-detached dwellings and two detached dwellings with associated parking, garages and access	Refused 6 May 2016
	14/01363/F	Demolition of houses and outbuildings. Erection of three pairs of semi-detached dwellings and detached dwelling with associated parking, garages and access.	Refused Appeal dismissed 24 July 2015
	13/01262/F	Demolition of houses and outbuildings. Erection of four pairs of semi-detached properties with associated parking, garages and access.	Refused 24 January 2014

- 3.2 Application 15/02543/F was refused for the following two reasons:

The proposal, by virtue of the introduction of dwellings closer to the Langshott frontage, the lack of separation, extent of hard-surfacing and opportunity for soft landscaping along the access road and throughout the development, and the poor design detailing, would cumulatively result in significant diminishment and material harm to the verdant and spacious character of Langshott and also the setting of nearby Grade II listed buildings, contrary to policies Ho9, Ho13, Ho14, Ho16 and Pc9 of the Reigate and Banstead Borough Local Plan 2005, the Reigate and Banstead Local Distinctiveness Design Guide and the National Planning Policy Framework 2012.

The proposal fails to provide an agreed contribution to fund affordable housing provision within the Borough of Reigate & Banstead, and is therefore contrary to policy CS15 of the Reigate and Banstead Core Strategy 2014 and the Affordable Housing SPD.

4.0 Proposal and Design Approach

- 4.1 The proposed development seeks planning permission for the demolition of the existing semi-detached dwellings (Kerriemuir and Wheatridge) and their replacement with a part 1.5 storeys and part 2 storey block containing eight apartments.
- 4.2 A new shared surface access road would be created along the eastern side of the site, serving a 12 space parking court to the rear. An additional two parking spaces are sited to the front of the building. Landscaped communal grounds would be created to the front and rear of the building.
- 4.3 The application follows a number of previous applications on the site for schemes comprising of semi-detached and detached housing. This application seeks to overcome the objections to the previous applications in respect of design and character through – in particular – creation of a single building (not extending buildings into the rear of the site) on a revised building line.
- 4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.5 Evidence of the applicant's design approach is set out below:

Assessment	The site consists of two semi-detached dwellings in large plots. The area within which the site sits is identified in the Horley Design Guide as contributing to the setting of listed buildings but the buildings themselves are not identified as being worthy of retention. Grade II listed buildings are located east and west
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	<p>of the site and a pair of traditional Surrey-style dwellings adjoin the site immediately to the west. These buildings are fairly reasonably spaced and of lower build to plot ratio. To the north and immediate east of the site is the new development of The Acres in which buildings are closer space and within smaller plots.</p> <p>There are two large Oak trees in the front garden of Kerriemuir subject to a preservation order. These are to be retained. Front hedging is also proposed to be retained where possible.</p>
Involvement	Pre-application advice was sought from the Council on the proposals prior to submission.
Evaluation	The Design & Access Statement explains the evolution of the scheme through and as a result of the various previous refused applications and appeal. The DAS sets out the design response to each of the issues raised including reinstatement of a staggered building line better respecting Langshott, a single block of apartments as opposed to houses to avoid introducing buildings on the rear of the site, increasing spacing to boundaries, responding to local vernacular including cues from Tanyard Farmhouse and The Farmhouse Public House and strengthening landscaping.
Design	The applicant's justification for the chosen design is that the proposed building footprint, building line and design detailing seek to overcome the previous reasons for refusal. Switching from a scheme of houses to flats avoids the need for buildings on the rear of the site.

4.6 Further details of the development are as follows:

Site area	0.22ha
Existing use	Residential
Proposed use	Residential
Net increase in dwellings	6
Proposed site density	36 dwellings per hectare (dph)
Density of the surrounding area	13dph – Langshott (public house to site) 22dph – The Fieldings (south of Langshott) 36dph – Flint Close/Newman Road (The Acres)
Proposed parking spaces	14 (12 allocated, 2 visitors)
Parking standard	BLP 2005 – 12 (1.5 per 2 bed unit)
Estimated CIL contribution	c.£38,700

5.0 Policy Context

5.1 Designation

Urban Area
Tree Preservation Order – RE1245

5.2 Reigate and Banstead Core Strategy

CS1(Presumption in favour of sustainable development)
CS4 (Valued townscapes and historic environment)
CS10 (Sustainable development)
CS11 (Sustainable construction)
CS13 (Housing delivery)
CS14 (Housing needs of the community)
CS15 (Affordable housing)
CS17 (Travel options and accessibility)

5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc4
Heritage	Pc9, Pc10
Housing	Ho9, Ho13, Ho16
Movement	Mo5,Mo6, Mo7
Utilities	Ut4

5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance	Developer Contributions SPD Affordable Housing SPD Local Distinctiveness Design Guide Surrey Design
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Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 (as amended) Conservation of Habitats and Species Regulations 2010
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6.0 Assessment

6.1 The application site is situated in the urban area of Horley, adjacent to the North East Sector (The Acres) development. There are two mature TPO oaks on the frontage of the site with Langshott and several listed buildings in the wider vicinity.

6.2 The previous applications and appeal decisions have established that there is no in principle objection to residential development of the site.

6.3 The main issues to consider are therefore:

- design and impact upon local character, including nearby listed buildings

- effects on the amenity of neighbouring properties
- access, parking and highway implications
- trees and landscaping
- CIL, infrastructure contributions and affordable housing
- other matters

Design and impact on the character of the area, including nearby listed buildings

- 6.4 The proposal would see the demolition of the existing semi-detached pair of dwellings and its replacement with a single building containing seven apartments.
- 6.5 The siting of the building follows a staggered building line which provides a transition between the adjoining cottages and more recently built dwellings on Flint Close. The layout of the building within the site provides a deep set back from the frontage of Langshott (c.12m) enabling generous landscaping and planting to be retained or introduced in front of the building, reinforcing the semi-rural character of the road. In this regard, the current proposal is considered to overcome concerns raised on previous schemes (including appeal schemes) which proposed new dwellings far closer to the road (set-back 6-7m) and thus which were far more disruptive to, and prominent within, the Langshott street scene.
- 6.6 Whilst wider than the existing semi-detached pair on the site, it is considered that the building would sit comfortably within the generous plot. Generous separation would be retained to both side boundaries as well as the frontage. Furthermore, there would be adequate space around the access road to the side of the dwelling to provide meaningful landscaping to soften this area and avoid a cramped or overly urban/suburban appearance which has been a concern on previous schemes. The area of hardstanding and car parking to the rear would be well screened and, unlike previous schemes, would not be prominent or conspicuous in the street scene. Overall, the layout would preserve and respect the informal, spacious and verdant character of Langshott.
- 6.7 Although a single block, the building does not appear conspicuous or out of keeping with the area. The overall scale and massing of the building is broadly reflective of some of the more imposing listed buildings in the locality, such as Tanyard Farmhouse and The Farmhouse pub. The form of the building (with symmetrical gables joined by a lower central element) is such that it would read – to some extent – as a semi-detached pair, similar to neighbouring dwellings. Whilst the building would be higher (eaves and ridge) than adjoining cottages, this has been reduced by over 0.5m during the course of the application to provide a more coherent roof profile and a transition in height between the traditional buildings on Langshott and the taller detached dwellings on Flint Close.
- 6.8 The replacement building, in term of appearance and detailing, is considered to be well-designed, appropriate to its surroundings and consistent with advice in the Horley Design Guide SPD incorporating steep roof pitch, gable features, elements of decorative tile hanging and large, well-positioned chimney stacks. The side elevation to the west (which would be partially visible in westward views along Langshott) has been well-designed and articulated to break up its depth.

- 6.9 To the rear, the building would be set within generous landscaped communal gardens, alongside the rear parking court. A car barn would be introduced to part of the parking court. A timber bin store would be sited to the front of the building: this would be a structure of modest height and it is considered that it would be adequately screened by frontage landscaping such that it would not be prominent or harmful to the street scene. The Conservation Officer has advised a condition requiring a hedge to be planted and maintained at the height of the bin store and this is considered reasonable.
- 6.10 The Conservation Officer has reviewed the proposals and considers the scheme to be an acceptable solution and that, subject to conditions requiring specific materials and details to ensure a high quality development; it would address previous concerns in respect of impact on the character of Langshott and setting of listed buildings in the locality.
- 6.11 Overall, whilst the proposed building would be larger than that which it replaces, its design, scale and massing, and the layout of the site are considered to reflect the spacious, verdant, semi-rural character and appearance of Langshott and the Wealden vernacular that typifies Horley. In doing so, the proposal would not adversely affect the setting of nearby listed buildings. The proposal is therefore considered to comply with policies Pc9, Ho9, Ho13 and Ho16 of the 2005 Borough Local Plan, policies CS4 and CS10 of the Core Strategy and the Horley Design SPD.

Effects on the amenity of neighbouring properties

- 6.12 The proposed development has been assessed with regards to its impact on neighbouring properties, including those on Langshott, Flint Close and Newman Road.
- 6.13 In terms of the neighbour to the west on Langshott, the proposal would retain adequate separation between the building and the shared boundary of c.3.4m. At this distance, and given the two storey scale, whilst the proposed building would be deeper than this neighbouring property it is not considered that the building would be overbearing on this neighbour, nor cause undue loss of light. In coming to this view, account has been taken of the fact that the dwelling on this neighbouring plot is some 8m from the boundary with the application site (with a garage intervening). Mature boundary screening – which is proposed to be retained – would also help to mitigate any impact even further. Both of the first floor windows facing towards this neighbour are proposed to be obscure glazed and this would be secured through condition to prevent overlooking.
- 6.14 To the east, the site adjoins no.16 Flint Close on the recently built North East Sector. The building itself would be generously separated from this boundary (by around 8m) due to the intervening access road. Given this and the siting of the block relative to this neighbour (i.e. it does not project materially beyond the rear elevation); it is not considered to give rise to overbearing or loss of light for the neighbour. First floor side facing windows would again be obscure glazed preventing risk of overlooking.

- 6.15 Whilst neighbour the proposal would introduce multiple occupancy and the rear elevation would have more windows than the present buildings, it is not considered that this would give rise to a relationship which is unusual for a residential area. Properties to the rear on Flint Close and Newman Road would be sufficiently distant from the building (over 35m to properties on Newman Road) such that the proposed flats would not materially overlook these neighbours.
- 6.16 The layout would introduce the access road along the boundary with several properties on Flint Close and the parking court would be to the rear of no.47 Newman Road. Whilst this would introduce a degree of activity (including vehicle movements) close to the gardens of the properties, the layout is considered to retain sufficient space alongside the parking area and access road to minimise any potential adverse impact on these neighbours. Any impact would be further ameliorated by the potential for meaningful landscaping to be introduced as well as the applicant's proposal for an acoustic fence along part of the eastern and rear boundaries. The Council's Environmental Health officer has advised that any acoustic fence should be of a minimum density of 10kg/m², this will be secured through condition. The car barn, due to its scale, siting and orientation in relation to neighbours is not considered to give rise to any adverse impacts.
- 6.17 Whilst some disturbance might arise during the construction process, this would by its nature be a temporary impact. Other environmental and statutory nuisance legislation exists to protect neighbours and the public should any particular issues arise. A condition requiring a Construction Transport Management Plan will assist in ensuring impacts of movements; parking and transport activity associated with the development are considered.
- 6.18 On this basis, whilst the current proposal would give rise to a degree of change in relationship to surrounding, there would not be a serious detriment to the amenity of these neighbours. The application is therefore considered comply with policies Ho9, Ho13 and Ho16 of the Borough Local Plan 2005.

Accessibility, parking and highway implications

- 6.19 The proposal would be served by a single access point from Langshott, situated to the eastern side of the frontage. An access road would run to the side of the proposed building serving a parking court in the rear of the site.
- 6.20 During the course of the application, and at the request of the County Highway Authority, the alignment of the crossover to Langshott was amended to reflect on-going improvements works which are being implemented along this road (including creation of a footway and passing points to facilitate use of the road by the Fastway bus service).
- 6.21 The County Highways Authority (CHA) was consulted upon the application and; having assessed it highway safety, capacity and policy grounds, has raised no objection subject to imposition of a number of conditions. Whilst local concerns regarding the nature of Langshott are acknowledged, the CHA have reviewed the proposals, including the design and likely intensity of use of the access, and have confirmed that they do not consider that the proposals would harm highway safety

or operation – even taking account of the fact that bus services are proposed to run along Langshott. Under the on-going improvement works, a “passing place” is proposed directly outside the site (which would be subject to double yellow lines to prevent parking): this would assist in ensuring that there is adequate space for cars to enter and exit the site, and for servicing of the development (e.g. refuse collections), without prejudicing safety or flow of traffic. The CHA has concluded that the volume of movements emerging from the development would be negligible and would not cause traffic or congestion, particularly when considered against existing flows on Langshott.

- 6.22 The development would provide a total of 14 parking spaces, 12 of which would be in the rear parking court with a further 2 to the front of the building. This parking would more than meet the maximum standards in the Borough Local Plan (which advises 1.5 spaces per 2 bed unit – i.e. 12), thus leaving two surplus spaces for additional or visitor parking. As such, the parking provision is considered to be adequate and overcomes concerns on previous schemes on this site regarding risk of displacement on-street parking onto Langshott.
- 6.23 The original plans showed a bin store set within the building itself; however, due to a lack of turning space on-site for large vehicles, this may have resulted in refuse vehicles reversing out onto Langshott. To address this, the applicants now propose a bin store to the front of the building enabling the site to be serviced from the road. The CHA have confirmed that this arrangement is acceptable.
- 6.24 On this basis, the proposal is considered to comply with policy Mo5, Mo5 and Mo7 of the 2005 Borough Local Plan in respect of its access, parking and highway arrangements.

Trees and landscaping

- 6.25 The site presently has significant tree cover, particularly to the front, including two mature Oaks which are covered by a Tree Preservation Order (RE1245). The Tree Officer has commented that these two Oaks are visually significant, typical of the ‘Low Weald’ character and form an important part of the sylvan setting of Langshott.
- 6.26 Given the arboricultural interest on the site, the application was supported by an Arboricultural Implications Assessment and initial method statement. This identifies that a number of trees and hedges would be removed; however, all are classified as being of lower ‘C’ category and typically self-sown. These losses are considered by the Tree Officer to be justified and it is considered that the scheme provides adequate opportunity for replacement tree planting, hedging and other soft landscaping to be introduced to compensate for this and maintain the verdant character. A landscape plan was submitted demonstrating that a high quality landscaping scheme can be achieved; however, due to minor changes in the layout, this requires revision prior to implementation and will be secured through condition.
- 6.27 The submitted arboricultural report also provides an initial indication of the measures to be employed to protect the retained trees. The Tree Officer has reviewed this and considered that it is sufficient to reach a judgement that the development can be implemented without causing undue harm to retained trees.

However, the Tree Officer has advised that revised final versions of these documents will be required prior to commencement in order to take account of amendments to the scheme (i.e. the realigned access) and installation of underground service routings which are not finalised at this stage. Given the importance of the two oaks, this condition, along with a condition requiring supervision, is considered reasonable to prevent damage or disturbance to root protection areas which could result in premature decline.

- 6.28 Accordingly subject to conditions requiring submission and implementation of a landscaping scheme and tree protection/supervision the proposal would not have an undue impact on the arboricultural interest of the site and has the potential to enhance the landscape character and visual amenity of the locality and would therefore comply with policy Pc4 and Ho9 of the Borough Local Plan 2005.

Community Infrastructure Levy (CIL), infrastructure contributions and affordable housing

- 6.29 As the proposals involve the creation of new dwellings, the development would be CIL liable. The exact amount of liability would be determined and collected after the grant of planning permission; however, based on the information provided by the applicant to date (and on the assumption that it can be proved that the existing buildings have been “in-use”, CIL liability in this case would be approximately £38,700.
- 6.30 In terms of other contributions and planning obligations, the Community Infrastructure Levy (CIL) Regulations which were introduced in April 2010 which states that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified.
- 6.31 Core Strategy Policy CS15 and the Council’s Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, in November 2014, the Government introduced policy changes through a Written Ministerial Statement (WMS) and changes to the national Planning Practice Guidance which restrict the use of planning obligations to secure affordable housing contributions from developments of 10 units or less and with a gross floor area of 1,000sqm or less. These changes were given legal effect following the Court of Appeal judgement in May 2016. This scheme falls within the scope of this exception.
- 6.32 In view of the above, and the resolution of the Planning Committee in November 2016, greater weight is therefore given to the national policy position in the WMS than the Council’s adopted policy. For this reason, it is not considered justified to seek contributions towards affordable housing in this case and the absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

Other matters

- 6.33 The proposal would make a positive contribution towards meeting the housing requirements of the borough and would provide homes for which there is a clear local need, with consequent social, economic and financial benefits. This is considered to add further, albeit modest, weight in favour of the proposal.
- 6.34 The site is within Flood Zone 1 according to the Environment Agency flood maps: it is therefore at low risk of flooding and is sequentially preferable for housing development. The site is also at very low risk of surface water flooding.
- 6.35 The site is not subject to any specific nature conservation designations and no specific habitat impact has been identified. As above, the majority of existing trees would be retained and new/replacement planting introduced which provide additional habitat and assist in ensuring there would not be undue harm to biodiversity. Concerns have been raised in representations regarding the presence of bats in the locality and within the site. No ecological information or bat survey has been submitted in support of the application; however, given the nature of the site and the existing buildings, it is considered that it has the potential to be a foraging and potential roosting environment. On this basis, and to comply with relevant legislation, it is considered that a condition requiring completion and submission of a bat survey (and any associated mitigation recommendations) prior to commencement of any works on site is necessary, relevant and reasonable to safeguard this protected species and habitats.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Arboricultural Plan	J44.83/02		07.08.2017
Survey Plan	4798 2 15		07.08.2017
Existing Plans	4798 2 16		21.08.2017
Site Layout Plan	4798 3 45	B	27.09.2017
Elevation Plan	4798 3 50	A	27.09.2017
Elevation Plan	4798 3 47	C	04.10.2017
Elevation Plan	4798 3 49	C	04.10.2017
Floor Plan	4798 3 46	A	04.10.2017

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

3. No development, including demolition or site clearance, shall commence until a Bat Survey, including any necessary measures for mitigation, compensation and enhancement has been carried out and submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in strict accordance with the details and measures when approved.

Reason:

To ensure that any protected species or the habitats thereof are safeguarded with respect to policy Pc2G of the Reigate and Banstead Borough Local Plan 2005, policy CS10 of the Reigate and Banstead Core Strategy and the provisions of the NPPF.

4. No development shall commence until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policies Pc13 and Ho9.

5. No development shall commence until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Details to be submitted for this condition should follow the specification below and there shall be no variation without the prior approval in writing of the Local Planning Authority:

- (a) All tiles and tile hanging shall be of handmade sandfaced plain clay tiles
- (b) All elevations shall be tile hung above ground floor level with alternating rows of fish scale and plain tiles and no Winchester cut.
- (c) All brickwork shall be handmade sandfaced red stock brick.
- (d) All external joinery shall be of painted timber with architraved bargeboards with no box ends.
- (e) All windows shall be of white painted timber with casements in each opening, with external glazing of traditional profile and where windows are in brickwork, set back behind the reveal at one brick depth, with gauged brick arches.
- (f) All fascias shall be no more than two bricks depth.
- (g) All rainwater goods shall be of painted cast metal or cast metal profile.
- (h) Before the chimney stack is commenced details of the chimney stack shall be submitted to and approved in writing by the LPA.
- (i) All footpaths and drives shall be of fixed gravel.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9, Ho13, Pc10, Pc12 and Pc13.

6. No development shall commence including demolition and or groundworks preparation until a revised scaled Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

7. No development shall commence on site until a scheme for the landscaping and replacement tree planting of the site including the retention of existing landscape features has been submitted and approved in writing by the local planning authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme. Hard landscaping details should reflect the requirements set out in Condition 1 above.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or use of the approved development or in accordance with a programme agreed in writing with the local planning authority.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan 2005 and the recommendations within British Standard 5837.

8. A mature hawthorn hedge shall be provided to the front boundary of the site with Langshott. The hedge shall consist of a staggered double row planted at 30cm

centres and shall have an initial planting height of at least 1m height, except in front of the bin store where the hedge should match the height of the bin store.

This hedge shall be planted prior to occupation of the development and shall thereafter be retained on an ongoing basis and managed to maintain a height of at least 1m or in the case of hedging in front of the bin store, at least the height of the bin store.

Any losses through death, disease or removal shall be remedied by replacement hawthorn, to current landscape standards, within 1 year to maintain this feature.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan 2005 and the recommendations within British Standard 5837.

9. No development shall commence until a Construction Transport Management Plan has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Such a plan must include details of:

- (a) parking for vehicles of site personnel, operatives and visitors
- (b) loading and unloading of plant and materials
- (c) storage of plant and materials
- (d) programme of works (including measures for traffic management)
- (e) provision of boundary hoarding behind any visibility zones
- (f) HGV deliveries and hours of operation
- (g) construction vehicle routing to and from the site
- (h) measures to prevent deposit of materials on the highway
- (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

10. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking/turning areas shall be retained and maintained for their designated purposes.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

11. The development hereby approved shall not be first occupied unless and until the proposed vehicular access to Langshott has been constructed and provided with a

visibility zone to the west in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. Thereafter, the visibility zone shall be kept permanently clear of any obstruction over 1.0m high.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

12. The development hereby approved shall not be first occupied unless and until the access to the site from Kerriemuir to Langshott has been permanently closed and any kerbs, verge, footway fully reinstated.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

13. The development hereby approved shall not be first occupied unless and until the proposed bin store and internal bike store have been provided in accordance with the approved plans. These facilities shall thereafter be retained and maintained for their designated purpose.

Reason:

In order that the development should make adequate provision for waste and recycling and to encourage sustainable travel in accordance with policy Ho9 of the Reigate and Banstead Borough Local Plan 2005 and policy CS17 of the Reigate and Banstead Core Strategy.

14. The development hereby approved shall not be first occupied unless and until the proposed acoustic fence has been installed along the boundary of the site in the location shown on approved plan 4798 3 45 Rev B. The acoustic barrier shall be continuous, with a minimum height of 2 metres and a minimum surface density of 10kg/m². The barrier shall be retained and maintained throughout the life of the development.

Reason:

To ensure that the development does not affect the amenity of neighbouring properties by noise and disturbance with regard to policies Ho9 and Ho13 of the Reigate and Banstead Borough Local Plan 2005.

15. The first floor windows in the east and west side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason:

To ensure that the development does not affect the amenity of neighbouring properties by overlooking with regard to policies Ho9 and Ho13 of the Reigate and Banstead Borough Local Plan 2005.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the occupation of the development, adequate provision should be made for waste storage and collection. You are advised to contact the Council's Recycling and Cleansing team to discuss the required number and specification of wheeled bins on rc@reigate-banstead.gov.uk or on the Council's website at http://www.reigate-banstead.gov.uk/downloads/file/2579/making_space_for_waste
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
6. When access is to be closed as a condition of planning permission and agreement with, or licence issued by, the Highway Authority Local Highways Service will require that the redundant dropped kerb be raised and any verge or footway crossing be reinstated to conform with the adjoining existing surfaces at the developers expense.

7. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that a permit and a Mini Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme.#>
8. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road marking, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
9. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above conditions. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
10. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
11. The applicant is advised to use the 'Secured by Design' scheme to design out crime by the use of effective crime prevention and security standards.

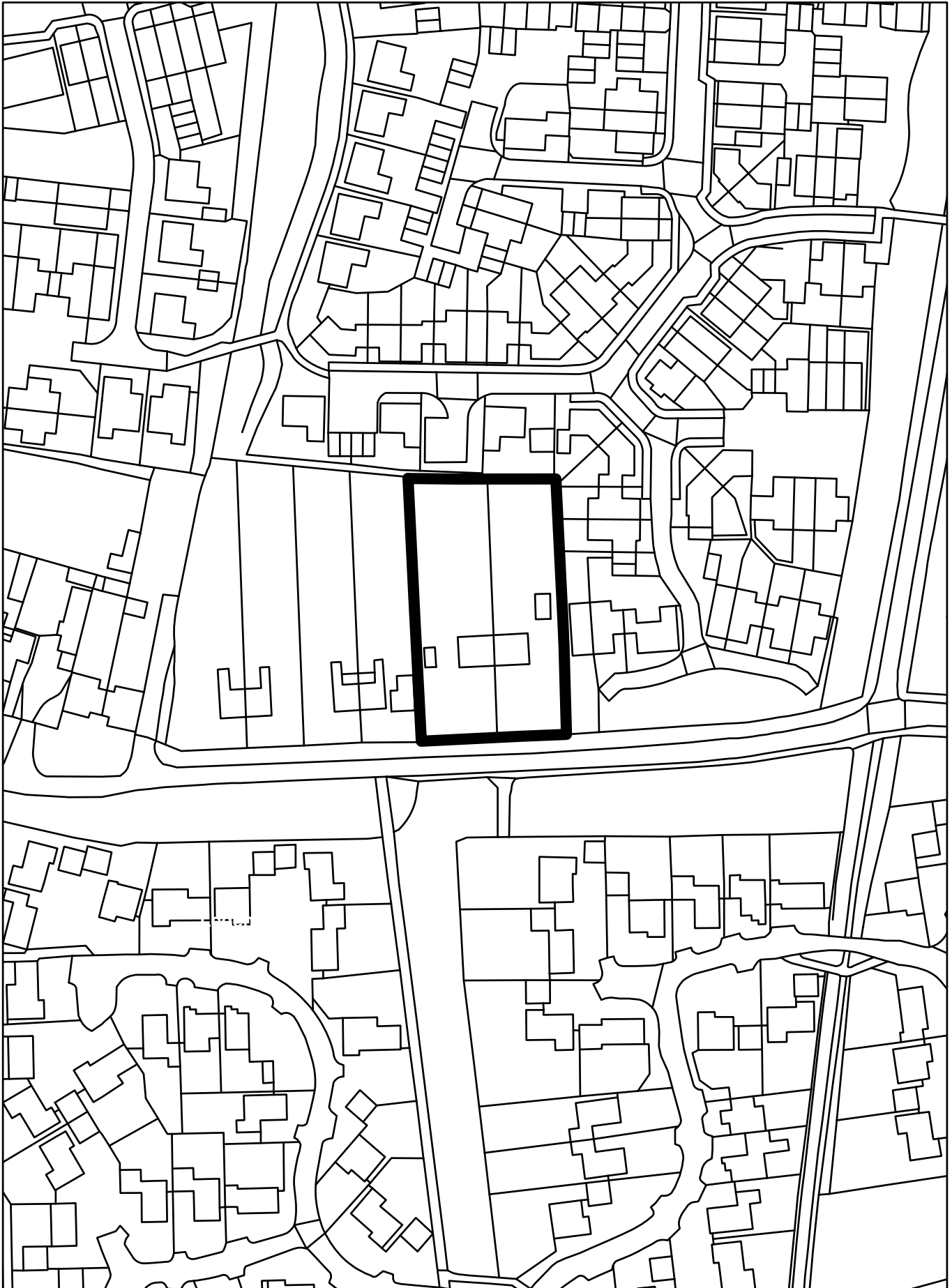
REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS10, CS11, CS13, CS14, CS15, CS17, Pc4, Pc9, Ho9, Ho13, Ho16, Mo5, Mo6, Mo7 and Ut4 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

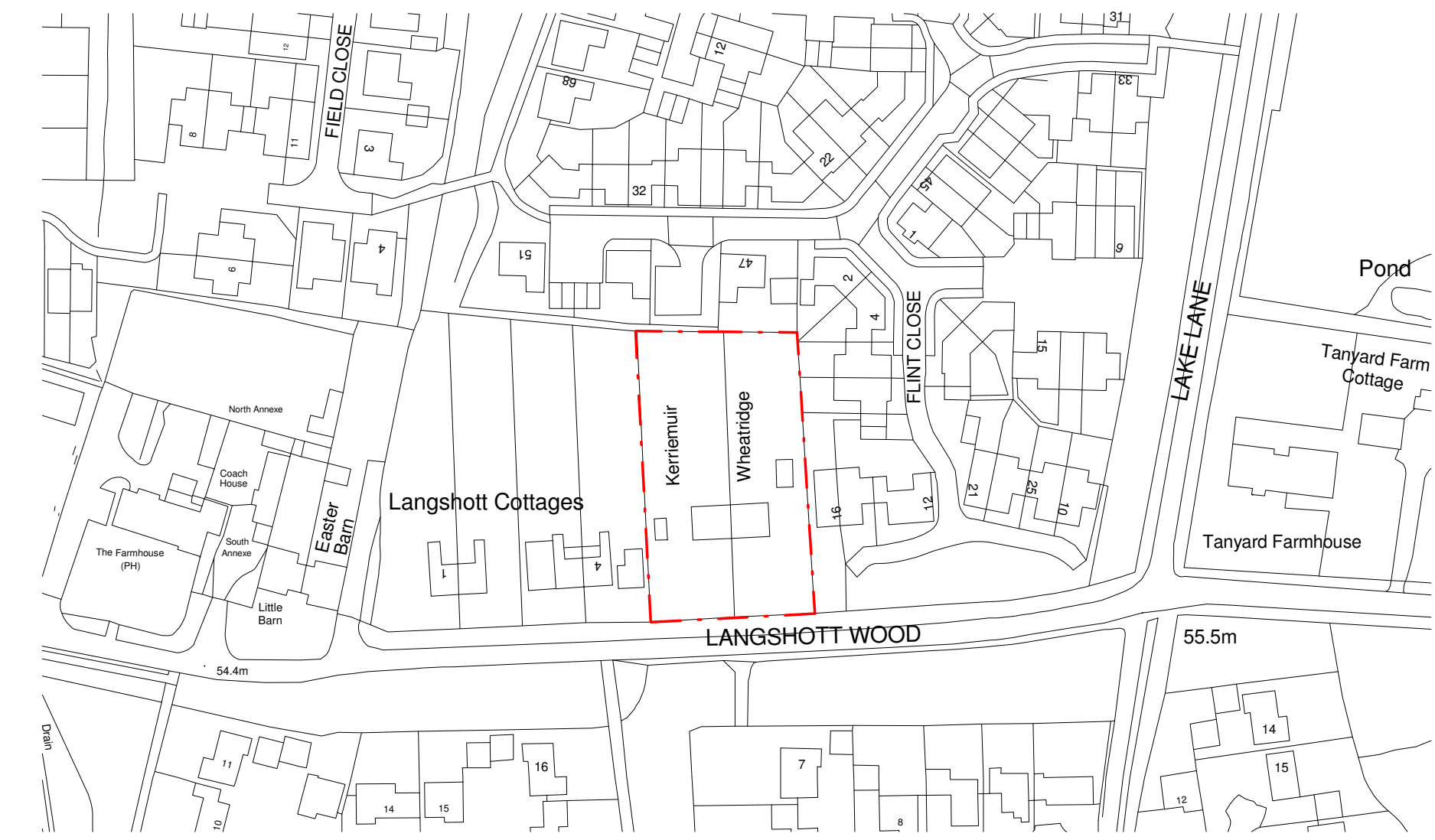
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

17/01839/F - Kerriemuir And Wheatridge, Langshott,
Horley





1 Site Plan
1 : 200



2 Site Location Plan
1 : 1250

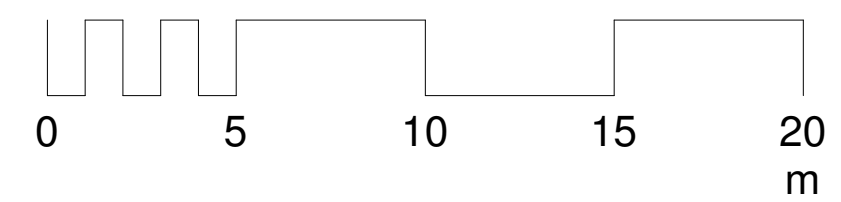
- Car Parking:**
 Flat 1 - 2 Bedroom = 1.5 Parking Spaces
 Flat 2 - 2 Bedroom = 1.5 Parking Spaces
 Flat 3 - 2 Bedroom = 1.5 Parking Spaces
 Flat 4 - 2 Bedroom = 1.5 Parking Spaces
 Flat 5 - 2 Bedroom = 1.5 Parking Spaces
 Flat 6 - 2 Bedroom = 1.5 Parking Spaces
 Flat 7 - 2 Bedroom = 1.5 Parking Spaces
 Flat 8 - 2 Bedroom = 1.5 Parking Spaces

Resident Spaces = 12
 Visitors Spaces = 2

Total Parking Spaces on Site = 14

Bike Store:
 Total Bicycle Spaces = 8

- - - - - Acoustic fence
- - - - - Removed trees
- - - - - Root protection area
- - - - - Outline of pre-application scheme: PAM/17/00077



PLANNING

Rev	Description	Issued	Dwn	Chk
B	Vehicular entrance adjusted to planning comments	27.09.17	RM	
A	Bin and bike store relocated and front vehicular entrance adjusted	26.09.17	RM	

Client
 Langshott Farms Ltd
 Project
 Langshott Cottages
 Langshott
 Horley, RH6 9LJ

Drawing
 Proposed
 Site Plan

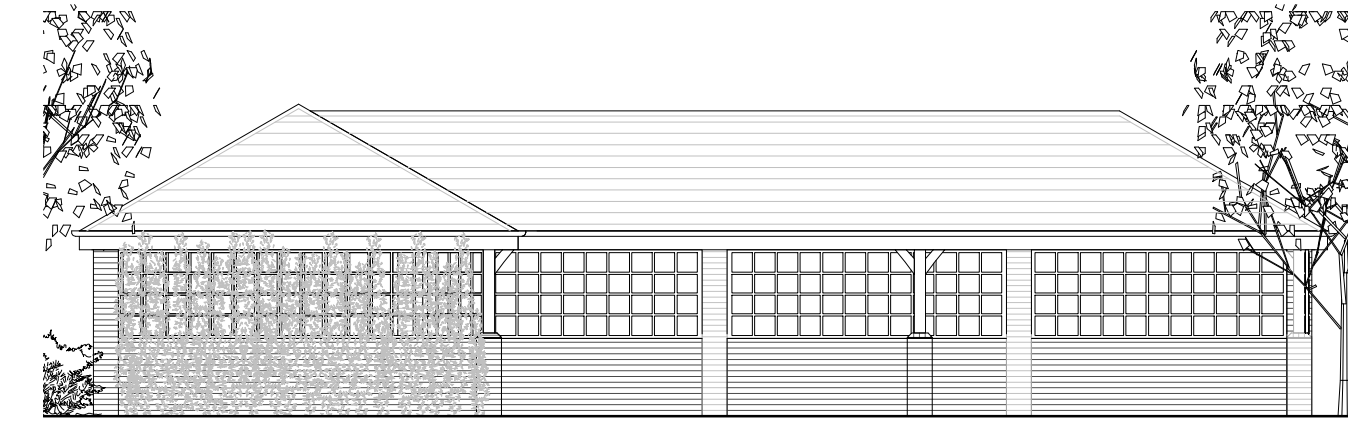
Scale	Date	Drawn	Checked
Various@A1	Jul '17	RM	IF

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 F 020 8876 4172
 E info@brookesarchitects.co.uk
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Rev. No.
4798 3 45 B



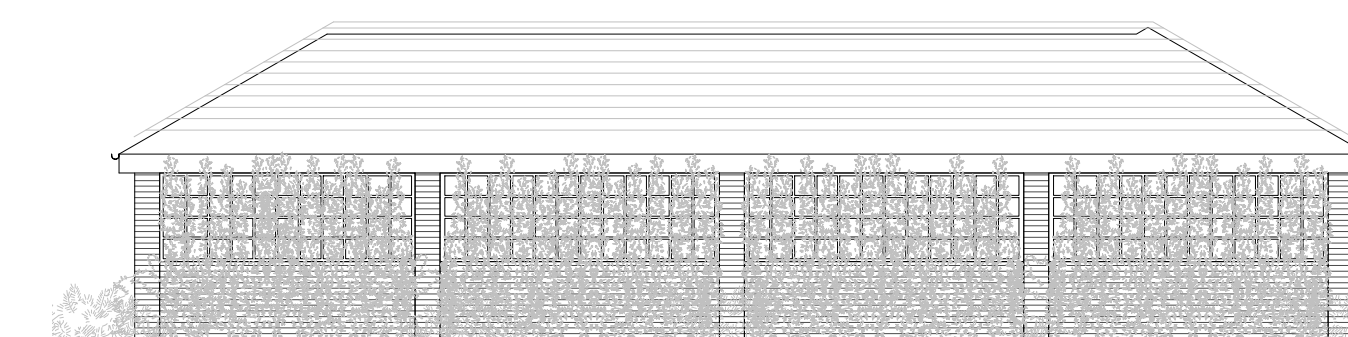
① Car Port Plan
1 : 100



② External Elevation A
1 : 100



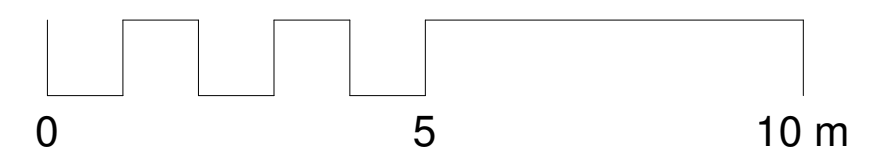
③ External Elevation B
1 : 100



④ External Elevation C
1 : 100



⑤ External Elevation D
1 : 100



PLANNING

Rev	Description	Issued	Dwn	Chk

Client
Langshott Farms Ltd

Project
Langshott Cottages
Langshott
Horley, RH6 9LJ

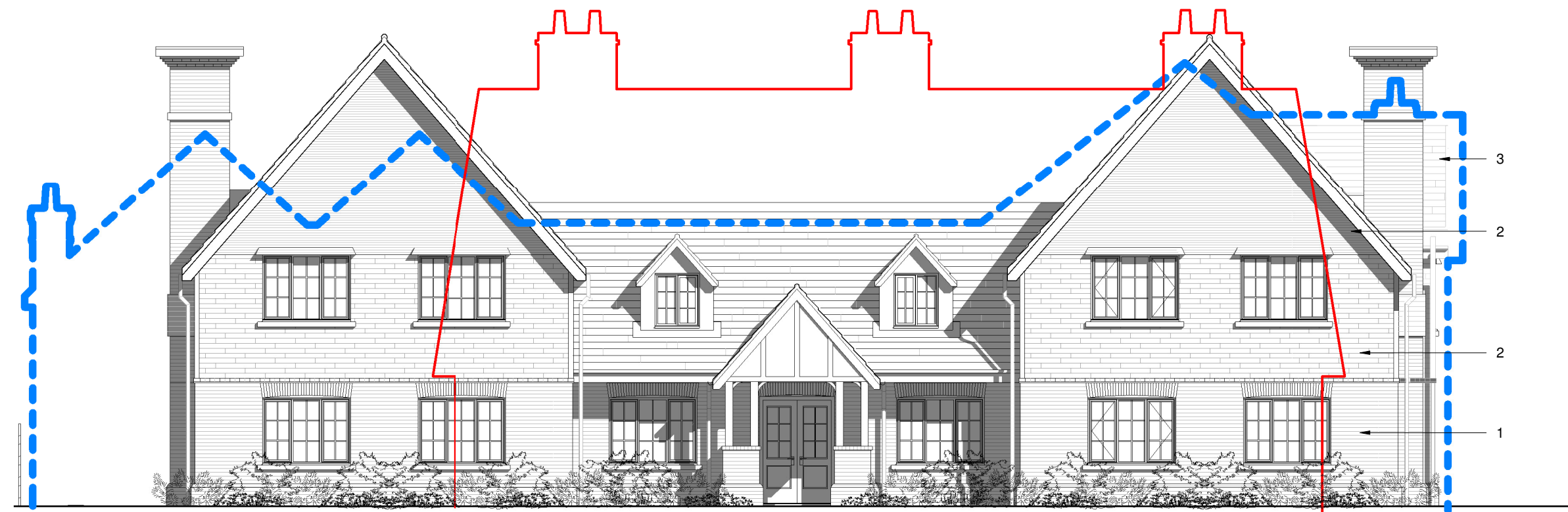
Drawing
Car Barn
Plans and Elevations

Scale	Date	Drawn	Checked
1 : 100@A1	Jul '17	RM	IF

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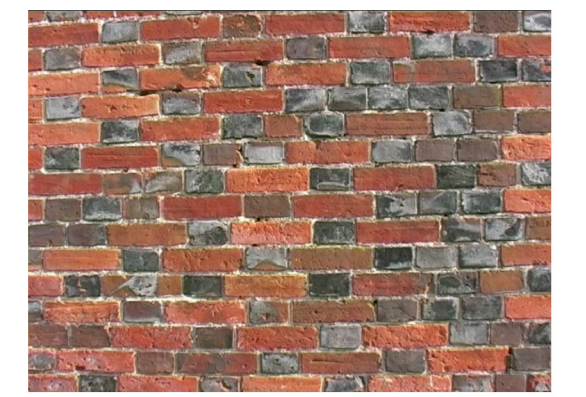
4798	3	48	Rev. No.
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① Front Elevation
1 : 100



② Side Elevation (West)
1 : 100



1. Flemish bond handmade red brickwork with burnt headers



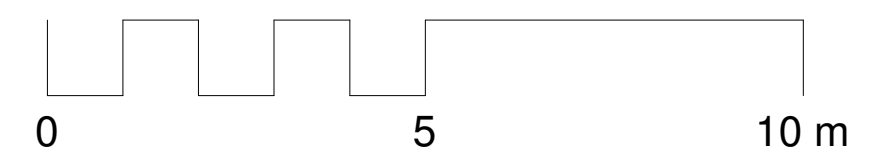
2. Decorative handmade clay tile hanging



3. Handmade sandfaced plain tiles with bonnet tiles to hips

— Existing house

- - - Outline of pre-application scheme: PAM/17/00077



PLANNING

Rev	Description	Date	Issued	Drawn	Checked
A	Elevations updated to planning comments	04.10.17		RM	

Client
Langshott Farms Ltd

Project
Langshott Cottages
Langshott
Horley, RH6 9LJ

Drawing
Proposed Elevations

Scale	Date	Drawn	Checked
1 : 100@A1	Jul '17	RM	IF

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③ Rear Elevation
1 : 100

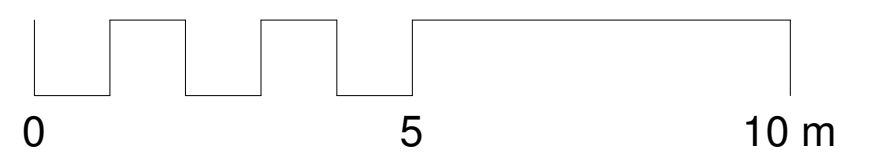


④ Side Elevation (East)
1 : 100



1 Street Elevation
1 : 100

— Existing house
- - - Outline of pre-application scheme: PAM/17/00077



PLANNING

Rev	Description	Issued	Dwn	Chk
C	Elevations updated to planning comments	04.10.17	RM	
B	Vehicular entrance adjusted to planning comments	27.09.17	RM	
A	Bin and bike store relocated and front vehicular entrance adjusted	26.09.17	RM	

Client
Langshott Farms Ltd

Project
Langshott Cottages
Langshott
Horley, RH6 9LJ

Drawing
Proposed
Street Elevation

Scale	Date	Drawn	Checked
1 : 100@A1	Jul '17	RM	IF

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Rev. No.
4798 3 49 C